04 - STRUCTURES
INDEX OF DRAWINGS

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THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA. APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.

Rabih M. Barakat P.E.
2014.12.12 11:00:12-05'00'

TRANSPORTATION PRINCIPAL ENGINEER

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-1_sb-00630702_Structure_Index_of_Drawings.dgn

SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

PROJECT TITLE:
PAVEMENT PRESERVATION
MILLING AND RESURFACING
OF INTERSTATE 91

TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

DRAWING TITLE:
STRUCTURES
INDEX OF DRAWINGS

PROJECT NO. 63-702
DRAWING NO. S-1
SHEET NO.

GENERAL NOTES

SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 816 (2004), AND SUPPLEMENTAL SPECIFICATIONS DATED JULY 2014 AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 7TH EDITION, 2014, U. S. CUSTOMARY UNITS AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003).

REINFORCING BARS: REINFORCING BARS IN ALL CONCRETE RECONSTRUCTIONS SHALL CONFORM TO ASTM A615, GRADE 60, AND BE GALVANIZED IN ACCORDANCE WITH ASTM A767, CLASS 1. CONCRETE COVER TO BE 2 INCHES.

ASPHALTIC PLUG EXPANSION JOINT SYSTEM NOTES

1. A BRIDGING PLATE SHALL BE USED TO SPAN THE GAP BETWEEN TWO DECK ENDS OR THE JOINT BETWEEN A DECK END AND A CONCRETE APPROACH SLAB.
2. DISCONTINUE THE INSTALLATION OF THE BACKER ROD, BRIDGING PLATE AND LOCATING PIN WHERE THE APPROACH SLAB IS DISCONTINUED (TYPICALLY IN THE ROADWAY SHOULDERS). SEE ASPHALTIC PLUG EXPANSION JOINT SYSTEM SPECIAL PROVISION.
3. NEW STEEL BRIDGING PLATES SHALL HAVE A MINIMUM THICKNESS OF $\frac{1}{4}$". FOR JOINT OPENINGS THAT EXCEED 3" A $\frac{3}{8}$" THICK BY 12" WIDE PLATE WILL BE REQUIRED.
4. NO BRIDGING PLATE SHALL BE USED AT THE FOLLOWING LOCATIONS:
   A. JOINT BETWEEN A DECK END AND A CONCRETE APPROACH PAVEMENT
   B. WHERE A BRIDGE DECK END MEETS A BITUMINOUS APPROACH PAVEMENT
5. SAW-CUTS MADE 3 FT. EACH SIDE OF CENTERLINE OF JOINT WILL BE PAID AS "CUT BITUMINOUS CONCRETE PAVEMENT".
6. THE REMOVAL OF ALL EXISTING JOINT SYSTEMS AND BITUMINOUS CONCRETE WITHIN THE LIMITS SHOWN TO BE INCLUDED FOR PAYMENT UNDER THE ITEM "REMOVAL OF HMA WEARING SURFACE".
7. INSTALLATION OF MEMBRANE WITHIN THE LIMITS SHOWN TO BE PAID FOR UNDER THE ITEM, "MEMBRANE WATERPROOFING (GLASS WOVEN FABRIC)"
8. CRACK SEALANT PLACED ALONG VERTICAL FACES OF THE SAW-CUT PAVEMENT TO BE PAID FOR UNDER THE ITEM, "JOINT AND CRACK SEALING OF BITUMINOUS CONCRETE PAVEMENT".
9. THE FURNISHING AND PLACING OF HMA S0.375 TO BE INCLUDED FOR PAYMENT UNDER ITEM THE ITEM "HMA S0.375".
10. SAW-CUTTING AND REMOVAL OF PAVEMENT FOR JOINT INSTALLATION TO BE INCLUDED FOR PAYMENT UNDER THE ITEM, "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".
11. CLOSED CELL BACKER ROD DIAMETER SHALL BE DETERMINED AFTER MEASURING THE JOINT OPENING. THE ROD SHALL BE 25% LARGER THAN THE JOINT OPENING.
12. ASPHALTIC PLUG EXPANSION JOINT SYSTEMS MAY BE INSTALLED ONLY WITHIN THE TEMPERATURE RANGE SPECIFIED IN THE SPECIAL PROVISION "ASPHALTIC PLUG EXPANSION JOINT SYSTEM". REFERENCE THE RANGE OF THERMAL MOVEMENT FOR THE SELECTED JOINT PRODUCT IN THE TABLE FOR "ALLOWABLE BRIDGE SUPERSTRUCTURE SURFACE TEMPERATURE RANGE" IN THE SPECIAL PROVISION.

REPLACE JOINT SEAL NOTES

1. PRIOR TO INSTALLING THE SILICONE SEALANT, CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK SHALL BE PAID FOR UNDER THE ITEM "REPLACE JOINT SEAL".
2. BRIDGE NO. 05882: USE THE PREFORMED JOINT SEAL BY SILICOFLEX BY R. J. WATSON, INC. OR V-SEAL BY D. S. BROWN, COMPANY.
3. BRIDGE NO. 01687: USE PREFORMED JOINT SEAL BY EMSEAL JOINT SYSTEM, LTD.

NOTICE TO CONTRACTOR
APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS

1. ALL THE REQUIREMENTS OF SPECIAL PROVISION SECTION 4.06 IN THE CONTRACT SHALL BE MET EXCEPT AS DESCRIBED BELOW.
2. THE BITUMINOUS CONCRETE MATERIAL SHALL BE PLACED AT A COMPACTED THICKNESS OF NO LESS THAN 1 ¼ INCHES TO A MAXIMUM OF 2 ½ INCHES. IF LIFTS OF VARYING THICKNESS ARE REQUIRED, THEY SHALL BE CONTAINED IN THE INTERMEDIATE LIFTS. THE FINAL LIFT SHALL BE OF UNIFORM THICKNESS. IN LIEU OF DENSITY TESTING, THE METHODS DESCRIBED BELOW SHALL BE FOLLOWED TO ASSURE PROPER COMPACTION.
3. BITUMINOUS CONCRETE MATERIAL SHALL BE PLACED AND SPREAD IN THE PREPARED AREA WITH COMPACTION COMMENCING PRIOR TO THE MATERIAL COOLING TO A TEMPERATURE OF 260°F. WHEN ANY BITUMINOUS CONCRETE MATERIAL IS NOT ABLE TO BE PLACED BEFORE REACHING THE MINIMUM DELIVERY TEMPERATURE OF 265°F IT SHALL BE PROPERLY DISCARDED BY THE CONTRACTOR AT NO COST TO THE STATE.
4. THE BITUMINOUS CONCRETE MATERIAL SHALL BE COMPACTED BY ALL AREAS RECEIVING THE MINIMUM NUMBER OF PASSES REQUIRED IN TABLE A BEFORE IT COOLS TO A TEMPERATURE OF 180°F. ALL COMPACTION (COMPLETING THE MINIMUM NUMBER OF SPECIFIED PASSES) SHALL BE COMPLETED BEFORE THE BITUMINOUS CONCRETE COOLS TO A TEMPERATURE OF 180°F. THE CONTRACTOR SHALL USE THE NUMBER OF COMPACTING EQUIPMENT NECESSARY TO COMPLETE THE PROCEDURE AS REQUIRED.
5. ALL INTERMEDIATE (NON-SURFACE) LIFTS SHALL BE COMPACTED WITH AN ASPHALT VIBRATORY PLATE COMPACTOR.
   a. THE VIBRATORY PLATE COMPACTOR SHALL MEET THE FOLLOWING REQUIREMENTS:
      i. IT SHALL BE DESIGNED TO COMPACT BITUMINOUS CONCRETE.
      ii. IT SHALL BE EQUIPPED WITH A WATER TANK.
      iii. IT SHALL GENERATE A CENTRIFUGAL FORCE OF AT LEAST 3200 POUNDS BUT NO GREATER THAN 6000 POUNDS.
      iv. IT SHALL HAVE AN OPERATING WEIGHT (WITHOUT WATER) OF AT LEAST 160 POUNDS.
      v. IT SHALL GENERATE A MINIMUM OF 4400 VIBRATIONS PER MINUTE.
      vi. ANY CORNERS OR OTHER AREAS THAT CANNOT BE REACHED BY THE VIBRATORY PLATE COMPACTOR SHALL BE COMPACTED WITH A HAND TAMPER (APPROVED FOR USE BY THE ENGINEER) A MINIMUM OF 20 TIMES (FOR ANY GIVEN AREA) BEFORE THE MATERIAL TEMPERATURE DROPS TO 180°F.
6. THE FINAL (SURFACE) LIFT SHALL BE COMPACTED WITH A DOUBLE DRUM ROLLER.
   a. THE DOUBLE DRUM ROLLER SHALL MEET THE FOLLOWING REQUIREMENTS:
      i. IT SHALL BE DESIGNED TO COMPACT BITUMINOUS CONCRETE.
      ii. IT SHALL WEIGH 3 ½ TO 4 ½ TONS
7. THE CONTRACTOR MAY REQUEST TO USE ALTERNATE EQUIPMENT BY SUBMITTING A SUPPLEMENT TO THEIR QC PLAN DESCRIBING THE EQUIPMENT'S SPECIFICATIONS AND PLACEMENT PROCEDURES. THE EQUIPMENT AND PROCEDURES MUST BE APPROVED BY THE ENGINEER PRIOR TO THEIR USE.
8. IF THE ABOVE METHODS ARE NOT COMPLETED TO THE SATISFACTION OF THE ENGINEER, HE MAY REQUIRE THE DENSITY ANY LIFT OF 1 ½ INCHES OR GREATER BE VERIFIED BY USE OF A QUALITY CONTROL NUCLEAR DENSITY GAUGE SUPPLIED BY THE CONTRACTOR. IF DENSITY VERIFICATION IS REQUIRED BY THE ENGINEER THE VALUES MUST CONFORM TO THE REQUIREMENTS OF SPECIAL PROVISION SECTION 4.06 IN THE CONTRACT.

TABLE A

LIFT THICKNESS (IN.)	NUMBER OF PASSES
1 ¼ to 1 ½	8
GREATER THAN 1 ½ TO 2	10
GREATER THAN 2 TO 2 ½	12

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-2..sb..00630702..General. Notes.dgn

SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION MILLING AND RESURFACING OF INTERSTATE 91

TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

DRAWING TITLE:
GENERAL NOTES

PROJECT NO. 63-702
DRAWING NO. S-2
SHEET NO.

BRIDGE INFORMATION FOR THE INSTALLATION OF DECK EXPANSION JOINTS (HARTFORD AND WINDSOR)

BRIDGE NO.	BRIDGE DATA					ABUTMENT 1*				ABUTMENT 2**			
	MILE POINT	LOCATION	NO. OF TRAVEL LANES	SIDE WALKS	CURB-CURB WIDTH (FT.)	MAX. THERMAL EXAPANSION (IN.)	SKEW (DEGREES)	DECK JOINT DETAIL	PARAPET JOINT DETAIL	MAX. THERMAL EXAPANSION (IN.)	SKEW (DEGREES)	DECK JOINT DETAIL	PARAPET JOINT DETAIL
01687	38.59	I-91 & TR 841 OVER CT SOUTHERN RAILROAD, HARTFORD	7	NO	103.00	0.62	N/A	A/S-4	L/S-13	0.63	N/A	B/S-5	L/S-13
05862	39.21	I-91 SB OVER CT SOUTHERN RAILROAD, HARTFORD	4	NO	68.00	1.24	48.00	D/S-7	L/S-13	0.58	48.00	D/S-7	L/S-13
05863	39.21	I-91 NB OVER CT SOUTHERN RAILROAD, HARTFORD	4	NO	68.00	1.24	44.00	D/S-7	L/S-13	0.58	44.00	D/S-7	L/S-13
05864	39.48	I-91 NB OVER LIEBERT ROAD, HARTFORD	5	NO	80.00	0.62	12.00	D/S-7	L/S-13	0.62	12.00	D/S-7	L/S-13
05866	40.80	I-91 NB OVER WESTON STREET, HARTFORD	4	NO	84.00	0.00	3.00	D/S-7	L/S-13	0.77	3.00	D/S-7	L/S-13
05881A	41.13	I-91 NB OVER ROUTE 159, WINDSOR	4	NO	75.00	1.47	31.00	D/S-7	L/S-13	1.47	33.00	D/S-7	L/S-13
05881B	41.13	I-91 SB OVER ROUTE 159, WINDSOR	4	NO	75.00	1.47	31.00	D/S-7	L/S-13	1.47	32.00	D/S-7	L/S-13
05882	0.04	I-91 NB ON RAMP OVER ROUTE 159, WINDSOR	1	NO	26.00	0.00	4.00	C/S-6	L/S-13	1.30	29.00	G/S-10	N/S-14
05924	40.97	I-91 NB ON RAMP OVER AMTRAK RAILROAD, HARTFORD	5	NO	84.10	0.47	28.00	D/S-7	L/S-13	0.95	28.00	D/S-7	L/S-13
05976A	43.51	I-91 SB OVER ROUTE 178 (PARK AVENUE), WINDSOR	4	NO	75.00	0.00	10.00	C/S-6	L/S-13	1.12	10.00	C/S-6	L/S-13
05976B	43.51	I-91 NB OVER ROUTE 178 (PARK AVENUE), WINDSOR	4	NO	75.00	0.00	11.00	C/S-6	L/S-13	1.12	11.00	C/S-6	L/S-13
05977A	42.82	I-91 SB OVER ROOD AVENUE, WINDSOR	4	NO	75.00	0.95	26.00	D/S-7	L/S-13	0.00	26.00	C/S-6	L/S-13
05977B	42.82	I-91 NB OVER ROOD AVENUE, WINDSOR	5	NO	87.00	0.95	26.00	D/S-7	L/S-13	0.00	26.00	C/S-6	L/S-13
05979A	44.10	I-91 NB OVER CAPEN STREET, WINDSOR	4	NO	75.00	0.88	3.00	C/S-6	L/S-13	0.00	3.00	C/S-6	L/S-13
05979B	44.10	I-91 SB OVER CAPEN STREET, WINDSOR	4	NO	86.40	0.87	6.00	C/S-6	L/S-13	0.00	6.00	C/S-6	L/S-13
05994	39.48	I-91 SB OVER LIEBERT ROAD, HARTFORD	4	NO	68.00	0.62	12.00	D/S-7	L/S-13	0.62	12.00	D/S-7	L/S-13
05995	40.80	I-91 SB OVER WESTON STREET, HARTFORD	4	NO	97.00	0.00	3.00	D/S-7	L/S-13	0.77	3.00	D/S-7	L/S-13
06008	40.96	I-91 NB ON RAMP OVER AMTRAK RAILROAD, HARTFORD	4	NO	75.30	0.47	28.00	D/S-7	L/S-13	0.95	28.00	D/S-7	L/S-13
06040A	41.26	I-91 SB OVER KENEY PARK ROAD & MEADOW BROOK, HARTFORD	4	NO	75.00	1.04	36.00	D/S-7	L/S-13	1.04	36.00	D/S-7	L/S-13
06040B	41.26	I-91 NB OVER KENEY PARK ROAD & MEADOW BROOK, HARTFORD	5	NO	87.00	1.04	36.00	D/S-7	L/S-13	1.04	36.00	D/S-7	L/S-13
06151	0.71	I-91 SB ON RAMP OVER AMTRAK RAILROAD, HARTFORD	1	NO	24.00	0.46	23.00	D/S-7	L/S-13	0.95	23.00	D/S-7	L/S-13
06218	0.32	I-91 RAMP 204 OVER I-91 TR855, HARTFORD	1	NO	24.00	0.00	45.00	D/S-7	L/S-13	1.50	46.00	D/S-7	L/S-13
06219	0.61	I-91 TR 854 & 855 OVER I-291 RAMP 001, HARTFORD	2	NO	77.00	0.00	30.00	D/S-7	L/S-13	1.13	46.00	D/S-7	L/S-13

ABUTMENT 1 REFERS TO THE SOUTH ABUTMENT

ABUTMENT 2 REFERS TO THE NORTH ABUTMENT

*** JOINT 1 ON SHEET NO. HWY-04**

**** JOINTS 2 AND 3 ON SHEET NO. HWY-04**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

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Plotted Date: 12/12/2014

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STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-3..sb..00630702..Expansion..Deck..Joint..Installation..Table.dgn

SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION
MILLING AND RESURFACING
OF INTERSTATE 91

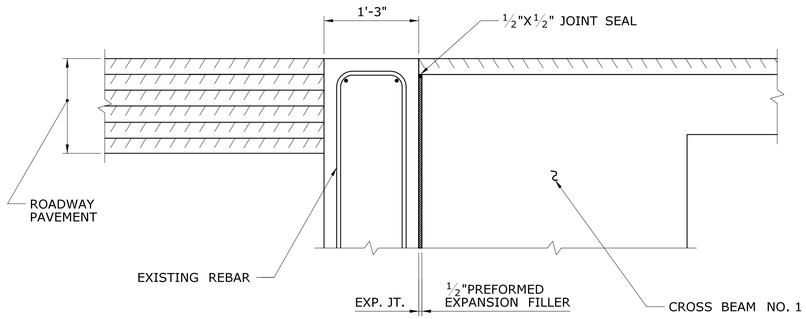
TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

DRAWING TITLE:
EXPANSION DECK JOINT
INSTALLATION TABLE

PROJECT NO. 63-702

DRAWING NO. S-3

SHEET NO.



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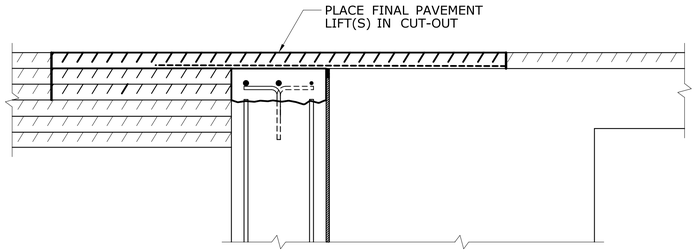
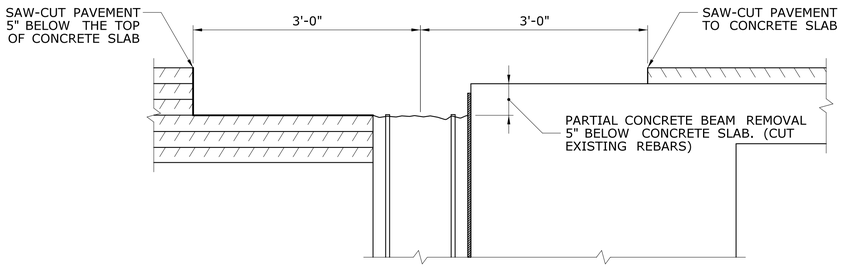
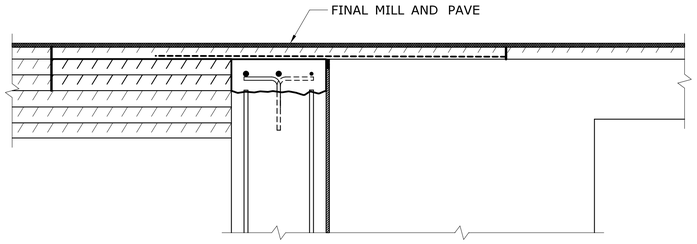


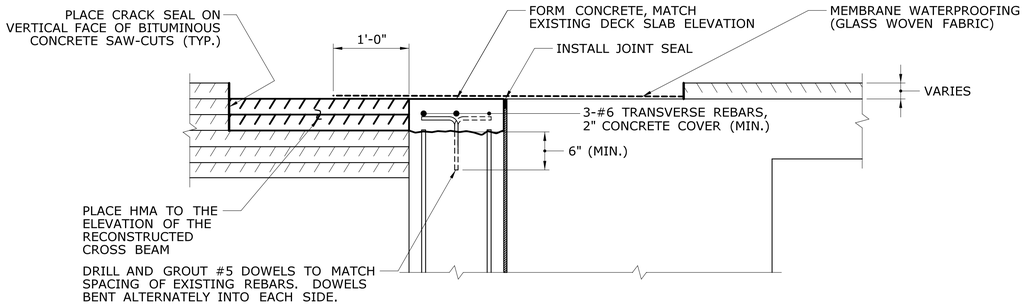
PLACE PAVEMENT IN CUT-OFF (STEP 9)



PAVEMENT AND PARTIAL BEAM REMOVAL (STEPS 1-3)



FINAL MILL AND PAVE (STEPS 10 AND 11)



BEAM RECONSTRUCTION (STEPS 4-8)

SUGGESTED SEQUENCE OF WORK

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION TO DETERMINE THE CENTER OF THE BEAM AT BOTH ENDS BEFORE PROCEEDING TO STEP 2.

STEP 2: SAW-CUT BITUMINOUS PAVEMENT TO THE DEPTHS SHOWN ON THE DETAILS. EACH SAW-CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE THE EXISTING DECK SLAB.

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND PARTIAL CONCRETE BEAM REMOVAL WITHIN THE LIMITS SHOWN ON THE DETAILS. REMOVAL OF PAVEMENT TO BE PAID FOR UNDER ITEM "REMOVAL OF HMA WEARING SURFACE". CONCRETE REMOVAL, DRILLING AND GROUTING OF DOWELS AND STEEL REINFORCEMNT TO BE FOR UNDER ITEM "RECONSTRUCT CONCRETE DECK ENDS"

STEP 4: RECONSTRUCT CONCRETE BEAM TO MATCH THE ELEVATION OF THE ADJACENT SLAB AS SHOWN ON THE PLANS. CONCRETE REMOVAL, DRILLING AND GROUTING OF DOWELS, AND STEEL REINFORCEMeNT TO BE PAID FOR UNDER ITEM "RECONSTRUCT CONCRETE DECK ENDS".

STEP 5: INSTALL EMSEAL JOINT SEAL ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS AND REQUIREMENTS. TO BE PAID FOR UNDER ITEM " REPLACE JOINT SEAL".

STEP 6: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES. FOR STAGED CONSTRUCTION, SEE DETAIL 'K' ON SHEET S-13.

STEP 7: PLACE HMA S0.375 TO THE ELEVATION OF THE RECONSTRUCTED BEAM. EACH LIFT TO BE $1\frac{1}{4}$" TO $2\frac{1}{2}$" THICK. MATCH THE ELEVATION OF THE EXISITNG PAVEMENT. REFER TO GENERAL NOTES - APJ BITUMINIOUS CONCRETE PLACEMENT REQUIREMENTS.

STEP 8: INSTALL WATERPROOFING MEMBRANE (WOVEN GLASS FABRIC) WITHIN THE LIMITS SHOWN. SEE DETAIL 'J' ON SHEET S-13.

STEP 9: PLACE THE REMAINING LIFTS OF HMA S0.375 IN THE PAVEMENT "CUTOUT". THE FIRST PAVEMENT LIFT SHALL BE $1\frac{1}{4}$" THICK. ADDITIONAL LIFTS SHALL BE $1\frac{1}{4}$" TO $2\frac{1}{2}$" THICK. MATCH THE ELEVATION OF THE EXISTING PAVEMENT. (REFER TO SHEET S-2 - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS).

STEP 10: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.

STEP 11: PAVE TOP COURSE ON ROADWAY AND BRIDGE.

A / - **BRIDGE NO. 01687**

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
-	-	-	-
-	-	-	-
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NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-4_sb_00630702_Bridge No. 01687_Joint_Seal_Details 1.dgn

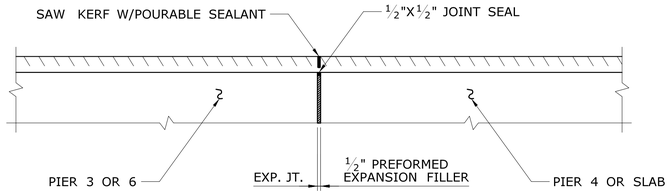
SIGNATURE/BLOCK: OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE: PAVEMENT PRESERVATION MILLING AND RESURFACING OF INTERSTATE 91

TOWN: CITY OF HARTFORD TOWN OF WINDSOR

DRAWING TITLE: BRIDGE NO. 01687 JOINT SEAL DETAILS 1

PROJECT NO. 63-702
DRAWING NO. S-4
SHEET NO.



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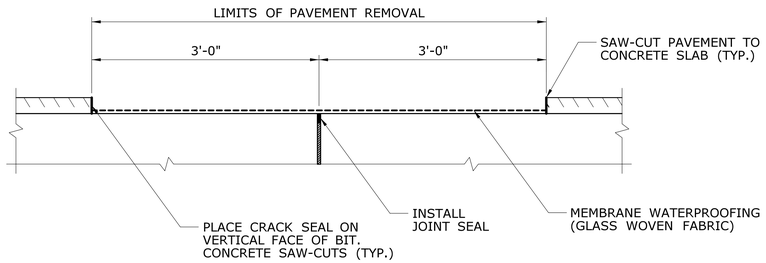
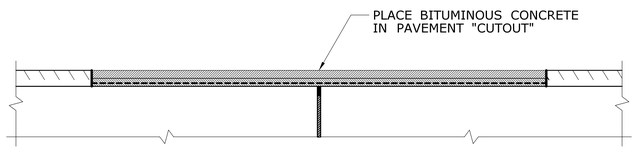
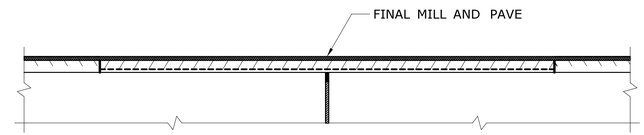


PAVEMENT REMOVAL (STEPS 1-6)



PLACE PAVEMENT IN "CUTOUT" (STEP 7)



FINAL MILL AND PAVE (STEP 9)

SUGGESTED SEQUENCE OF WORK

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION TO DETERMINE THE LOCATION OF THE EXPANSION JOINT BETWEEN THE GIRDERS AT PIERS 3 AND 4 AS WELL AS BETWEEN THE GIRDER AT PIER 6 AND THE ADJACENT SLAB.

STEP 2: SAW-CUT BITUMINOUS PAVEMENT TO THE CONCRETE SLAB. EACH SAW-CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. THE SAW-CUT SHALL NOT DAMAGE THE EXISTING DECK SLAB.

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL WITHIN THE LIMITS SHOWN ON THE PLANS. TO BE PAID FOR UNDER ITEM "REMOVAL OF HMA WEARING SURFACE".

STEP 4: INSTALL EMSEAL JOINT SEAL ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS AND REQUIREMENTS. TO BE PAID FOR UNDER ITEM "REPLACE JOINT SEAL".

STEP 5: PLACE CRACK SEALANT ON THE VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES. FOR STAGED CONSTRUCTION, SEE DETAIL 'K' ON SHEET S-13.

STEP 6: INSTALL WATERPROOFING MEMBRANE (WOVEN GLASS FABRIC) ON THE TOP OF THE DECK WITHIN THE LIMITS SHOWN. SEE DETAIL 'J' ON SHEET S-13.

STEP 7: PLACE HMA S0.375 IN THE PAVEMENT "CUTOUT". THE FIRST PAVEMENT LIFT SHALL BE $1\frac{1}{4}$" THICK. ADDITIONAL LIFTS SHALL BE $1\frac{1}{4}$" TO $2\frac{1}{2}$" THICK. MATCH THE ELEVATION OF THE EXISTING PAVEMENT. (REFER TO GENERAL NOTES - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS).

STEP 8: MILL AND PAVE ROADWAY.

B
-

BRIDGE NO. 01687

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ..\S-5_sb_00630702_Bridge No. 01687_Joint_Seal_Details 2.dgn

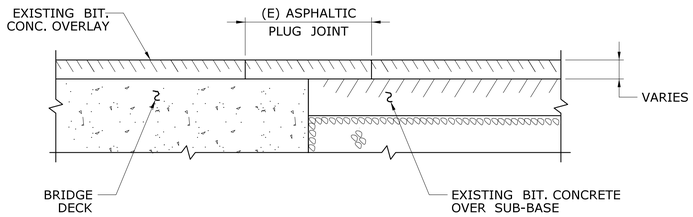
SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION
MILLING AND RESURFACING
OF INTERSTATE 91

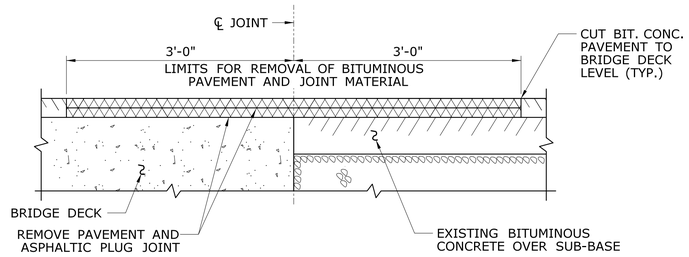
TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

DRAWING TITLE:
BRIDGE NO. 01687
JOINT SEAL DETAILS 2

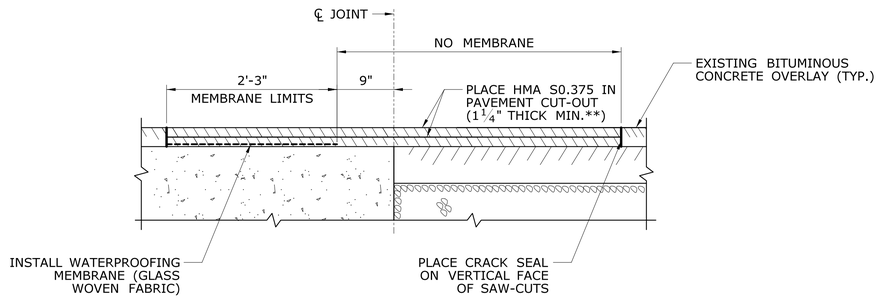
PROJECT NO. 63-702
DRAWING NO. S-5
SHEET NO.



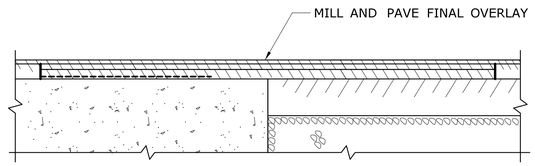
EXISTING CONDITION
(NO ROADWAY SLAB OR APPROACH SLAB EXISTS)



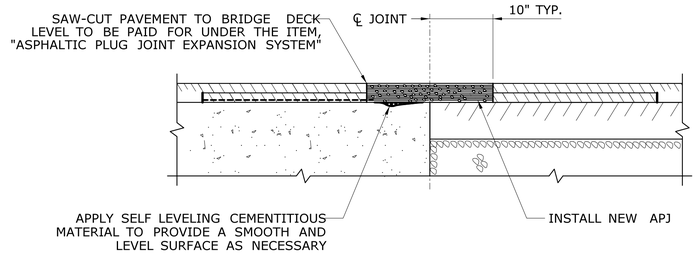
JOINT AND PAVEMENT REMOVAL
(STEPS 1-3)



PLACEMENT OF PAVEMENT IN JOINT CUTOUT
(STEPS 4-7)



MILLING AND PAVING
(STEP 8 & 9)



FINAL CONDITION
(STEPS 10 & 11)

SUGGESTED SEQUENCE OF WORK

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.

STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT SAW-CUT. EACH SAW-CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK OR APPROACH SLAB.

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL INCLUDING BACKING PLATE WITHIN THE LIMITS SHOWN. REMOVAL OF PAVEMENT TO BE PAID FOR UNDER ITEM "REMOVAL OF HMA WEARING SURFACE".

STEP 4: REPAIR DETERIORATED CONCRETE AS DIRECTED BY THE ENGINEER. WHEN CONCRETE REPAIRS ARE REQUIRED, DETAIL 'E' ON SHEET S-8 SHALL BE USED IN LIEU OF DETAIL 'C' ON THIS SHEET.

STEP 5: INSTALL WATERPROOFING MEMBRANE (WOVEN GLASS FABRIC) ON THE TOP DECK AND APRROACH SLAB WITHIN THE LIMITS SHOWN. SEE DETAIL 'J' ON SHEET S-13.

STEP 6: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES. FOR STAGED CONSTRUCTION SEE DETAIL 'K' ON SHEET S-13.

STEP 7: PLACE HMA S0.375 IN THE PAVEMENT "CUT-OUT". THE FIRST PAVEMENT LIFT SHALL BE 1¼" THICK. ADDITIONAL LIFTS SHALL BE 1¼" TO 2½" THICK. MATCH THE ELEVATION OF THE EXISTING PAVEMENT. (REFER TO GENERAL NOTES - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS).

STEP 8: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.

STEP 9: PAVE TOP COURSE ON ROADWAY AND BRIDGE.

STEP 10: CUT PAVEMENT FULL DEPTH AT 10" FROM THE CENTER OF THE JOINT (BOTH SIDES OF JOINT) AND REMOVE ALL PAVEMENT MATERIAL BETWEEN THE SAW-CUTS.

STEP 11: INSTALL FINAL ASPHALTIC PLUG JOINT SYSTEM.

SECTION - INSTALLATION OF ASPHALTIC PLUG JOINT WITHOUT BRIDGING PLATE (MILL & PAVE) — C -

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-6..sb..00630702..Asphaltic..Plug..Joint..Details..1.dgn

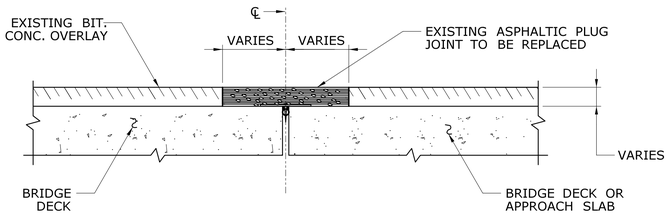
SIGNATURE/ BLOCK: OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE: PAVEMENT PRESERVATION MILLING AND RESURFACING OF INTERSTATE 91

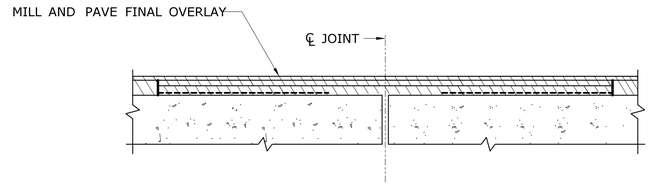
TOWN: CITY OF HARTFORD TOWN OF WINDSOR

DRAWING TITLE: ASPHALTIC PLUG EXP. JOINT SYSTEM DETAILS 1

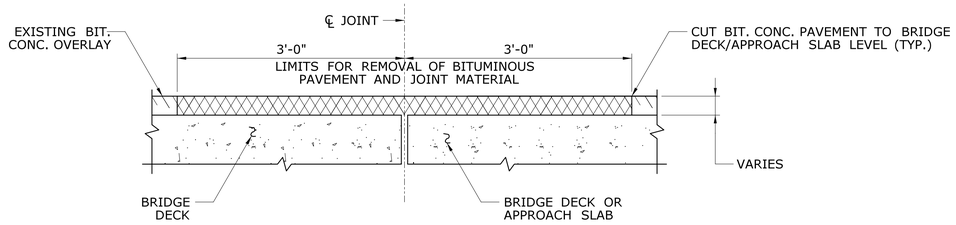
PROJECT NO. 63-702
DRAWING NO. S-6
SHEET NO.



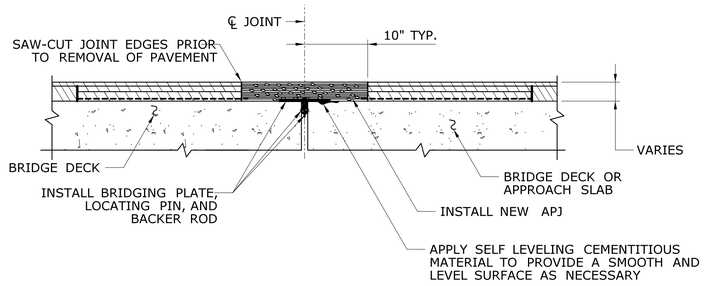
EXISTING CONDITION



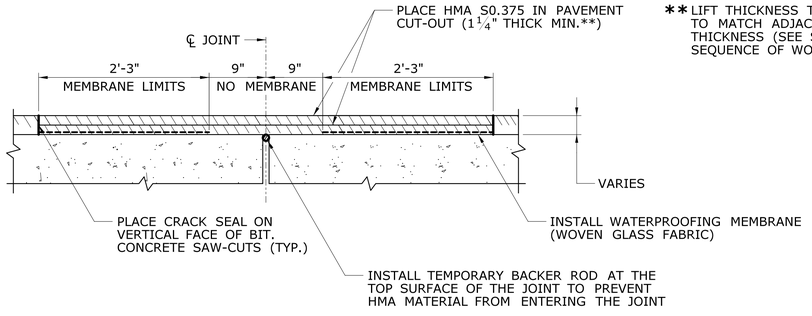
MILLING AND PAVING (STEPS 9 & 10)



JOINT AND PAVEMENT REMOVAL (STEPS 1-3)



FINAL CONDITION (STEPS 11 & 12)



PLACEMENT OF PAVEMENT IN CUTOUT
(STEPS 4-8)

SUGGESTED SEQUENCE OF WORK

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.

STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT SAW-CUT. EACH SAW-CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK OR APPROACH SLAB.

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL INCLUDING BACKING PLATE WITHIN THE LIMITS SHOWN.

STEP 4: INSTALL TEMPORARY BACKER ROD FLUSH WITH THE BRIDGE DECK AND APPROACH SLAB.

STEP 5: REPAIR ANY DETERIORATED CONCRETE AS DIRECTED BY THE ENGINEER. WHEN CONCRETE REPAIRS ARE REQUIRED, DETAIL 'F' ON SHEET 9 SHALL BE USED IN LIEU OF DETAIL 'D' ON THIS SHEET.

STEP 6: INSTALL WATERPROOFING MEMBRANE (WOVEN GLASS FABRIC) ON THE TOP OF THE DECK AND APRROACH SLAB WITHIN THE LIMITS SHOWN. SEE DETAIL 'J' ON SHEET S-13.

STEP 7: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES. FOR STAGED CONSTRUCTION, SEE DETAIL 'K' ON SHEET S-13.

STEP 8: PLACE HMA S0.375 IN THE PAVEMENT "CUT-OUT". THE FIRST PAVEMENT LIFT SHALL BE 1¼" THICK. ADDITIONAL LIFTS SHALL BE 1¼" TO 2½"THICK. MATCH THE ELEVATION OF THE EXISTING PAVEMENT. (REFER TO GENERAL NOTES - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS).

STEP 9: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.

STEP 10: PAVE TOP COURSE ON ROADWAY AND BRIDGE.

STEP 11: CUT PAVEMENT FULL DEPTH AT 10" FROM THE CENTER OF THE JOINT (BOTH SIDES OF JOINT) AND REMOVE ALL PAVEMENT MATERIAL BETWEEN THE SAW-CUTS.

STEP 12: INSTALL FINAL ASPHALTIC PLUG JOINT SYSTEM.

SECTION - INSTALLATION OF ASPHALTIC PLUG JOINT WITH BRIDGING PLATE — D / -

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

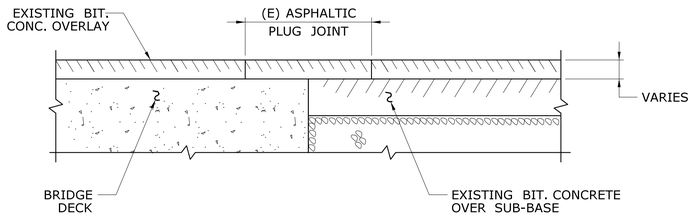
Filename: ...\S-7..sb..00630702..Asphaltic..Plug..Joint..Details..2.dgn

SIGNATURE/ BLOCK: OFFICE OF ENGINEERING
APPROVED BY:

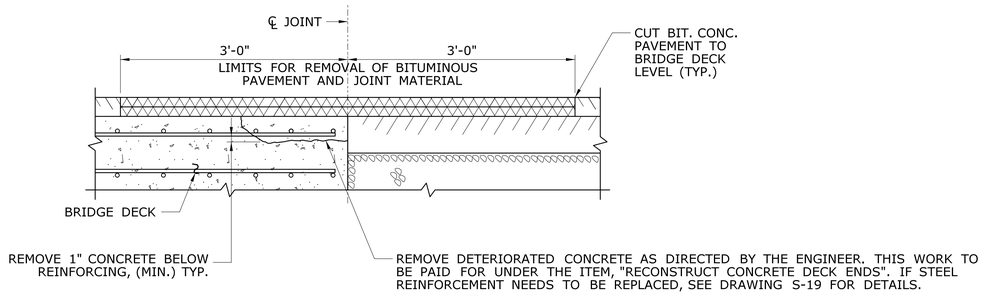
PROJECT TITLE: PAVEMENT PRESERVATION MILLING AND RESURFACING OF INTERSTATE 91

TOWN: CITY OF HARTFORD AND TOWN OF WINDSOR
DRAWING TITLE: ASPHALTIC PLUG EXP. JOINT SYSTEM DETAILS 2

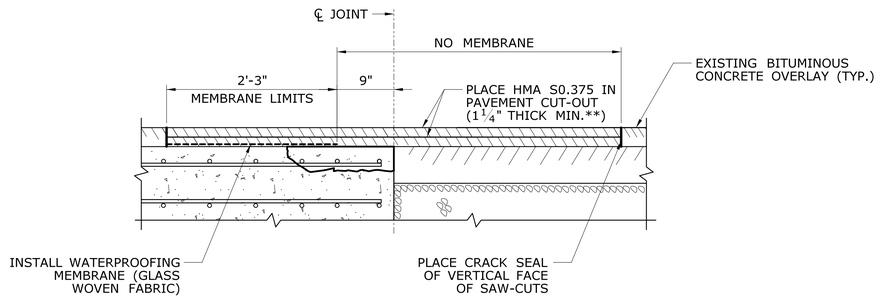
PROJECT NO. 063-702
DRAWING NO. S-7
SHEET NO.



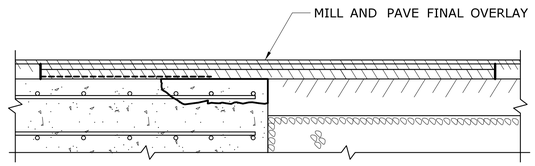
EXISTING CONDITION
(NO ROADWAY SLAB OR APPROACH SLAB EXISTS)



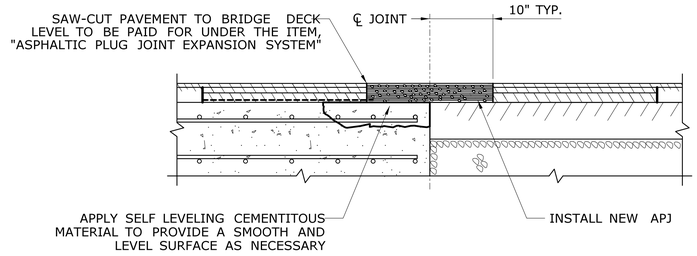
JOINT AND PAVEMENT REMOVAL
(STEPS 1-3)



PLACEMENT OF PAVEMENT IN JOINT CUTOUT
(STEPS 4-7)



MILLING AND PAVING
(STEP 8 & 9)



FINAL CONDITION
(STEPS 10 & 11)

SUGGESTED SEQUENCE OF WORK

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.

STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT SAW-CUT. EACH SAW-CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK OR APPROACH SLAB.

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL INCLUDING BACKING PLATE WITHIN THE LIMITS SHOWN.

STEP 4: REPAIR ANY DETERIORATED CONCRETE AS DIRECTED BY THE ENGINEER. WHEN THE STEEL REINFORCEMENT HAS SECTION LOSS IN EXCESS OF 20% OF THE CROSS-SECTIONAL AREA, REPLACE THE STEEL REINFORCEMENT IN ACCORDANCE WITH THE DETAILS AS SHOWN IN DETAIL 'H' ON SHEET S-13. REPAIR OF DETERIORATED CONCRETE TO BE PAID UNDER ITEM "RECONSTRUCT CONCRETE DECK ENDS".

STEP 5: INSTALL WATERPROOFING MEMBRANE (WOVEN GLASS FABRIC) ON THE TOP DECK AND APRROACH SLAB WITHIN THE LIMITS SHOWN. SEE DETAIL 'J' ON SHEET S-13.

STEP 6: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES. FOR STAGED CONSTRUCTION, SEE DETAIL 'K' ON SHEET S-13.

STEP 7: PLACE HMA S0.375 IN THE PAVEMENT "CUT-OUT". THE FIRST PAVEMENT LIFT TO SHALL BE 1¼" THICK. ADDITIONAL LIFTS SHALL BE 1¼" TO 2½" THICK. MATCH THE ELEVATION OF THE EXISTING PAVEMENT. (REFER TO GENERAL NOTES - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS).

STEP 8: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.

STEP 9: PAVE TOP COURSE ON ROADWAY AND BRIDGE.

STEP 10: CUT PAVEMENT FULL DEPTH AT 10" FROM THE CENTER OF THE JOINT (BOTH SIDES OF JOINT) AND REMOVE ALL PAVEMENT MATERIAL BETWEEN THE SAW-CUTS.

STEP 11: INSTALL FINAL ASPHALTIC PLUG JOINT SYSTEM.

SECTION - INSTALLATION OF ASPHALTIC PLUG JOINT WITHOUT BRIDGING PLATE (MILL & PAVE) — E -

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-8..sb..00630702..Asphaltic..Plug..Joint..Details..3.dgn

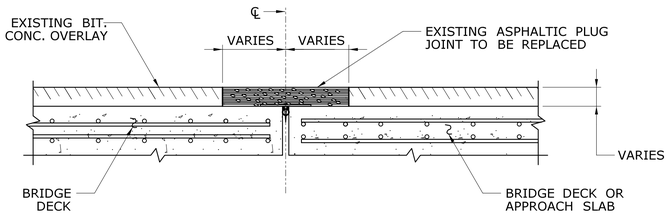
SIGNATURE/ BLOCK: OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE: PAVEMENT PRESERVATION MILLING AND RESURFACING OF INTERSTATE 91

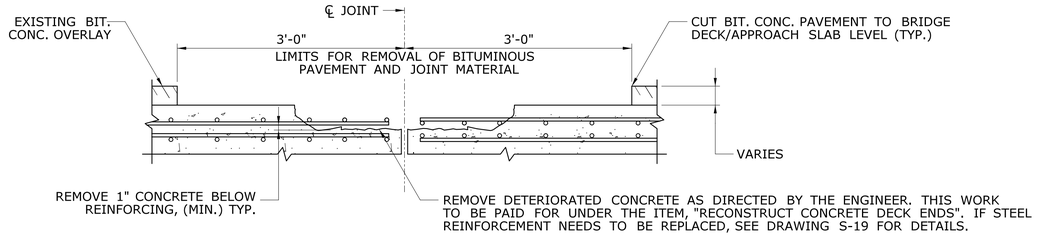
TOWN: CITY OF HARTFORD TOWN OF WINDSOR

DRAWING TITLE: ASPHALTIC PLUG EXP. JOINT SYSTEM DETAILS 3

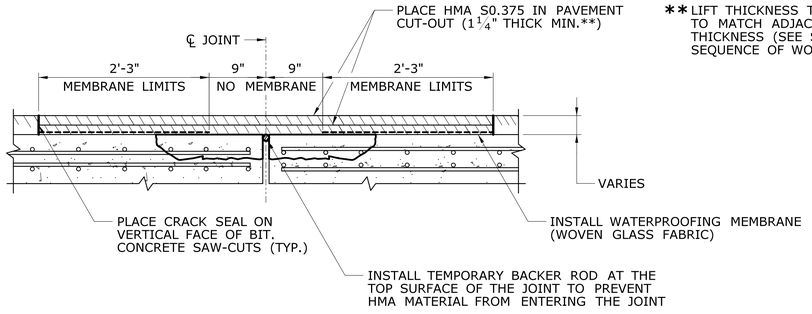
PROJECT NO. 63-702
DRAWING NO. S-8
SHEET NO.



EXISTING CONDITION

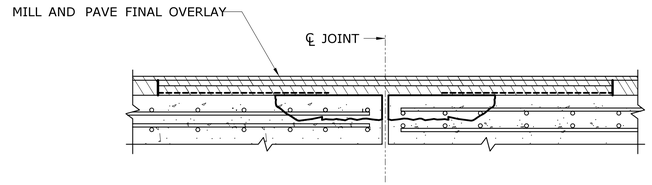


JOINT AND PAVEMENT REMOVAL (STEPS 1-3)

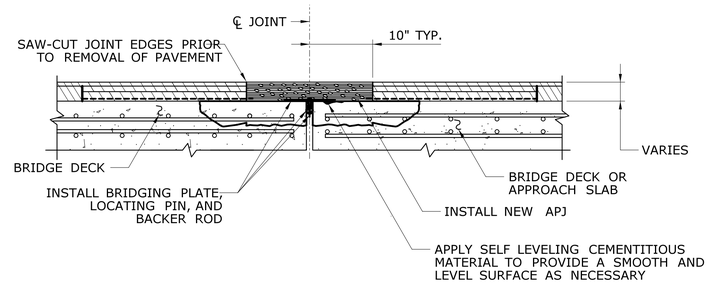


** LIFT THICKNESS TO BE ADJUSTED TO MATCH ADJACENT PAVEMENT THICKNESS (SEE STEP 6 OF SEQUENCE OF WORK)

PLACEMENT OF PAVEMENT IN CUTOUT (STEPS 4-8)



MILLING AND PAVING (STEPS 9 & 10)



FINAL CONDITION (STEPS 11 & 12)

SUGGESTED SEQUENCE OF WORK

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.

STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF EXISTING JOINT FOR PAVEMENT SAW-CUT. EACH SAW-CUT LINE SHALL BE 3' FROM THE CENTERLINE OF THE EXISTING JOINT. SAW-CUT SHALL NOT DAMAGE EXISTING DECK OR APPROACH SLAB.

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL AND JOINT MATERIAL INCLUDING BACKING PLATE WITHIN THE LIMITS SHOWN.

STEP 4: INSTALL TEMPORARY BACKER ROD FLUSH WITH THE BRIDGE DECK AND APPROACH SLAB.

STEP 5: REPAIR ANY DETERIORATED CONCRETE AS DIRECTED BY THE ENGINEER. WHEN THE STEEL REINFORCEMENT HAS SECTION LOSS IN EXCESS OF 20% OF THE CROSS-SECTIONAL AREA, REPLACE THE STEEL REINFORCEMENT IN ACCORDANCE WITH THE DETAILS AS SHOWN ON SHEETS S-11 AND S-12. REPAIR OF DETERIORATED CONCRETE TO BE PAID FOR UNDER ITEM "RECONSTRUCT CONCRETE DECK ENDS".

STEP 6: INSTALL WATERPROOFING MEMBRANE (WOVEN GLASS FABRIC) ON THE TOP OF THE DECK AND APRROACH SLAB WITHIN THE LIMITS SHOWN. SEE DETAIL 'J' ON SHEET S-13.

STEP 7: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES. FOR STAGED CONSTRUCTION, SEE DETAIL 'K' ON SHEET S-13.

STEP 8: PLACE HMA S0.375 IN THE PAVEMENT "CUT-OUT". THE FIRST PAVEMENT LIFT TO SHALL BE 1¼". ADDITIONAL LIFTS SHALL BE 1¼" TO 2½". MATCH THE ELEVATION OF THE EXISTING PAVEMENT. (REFER TO GENERAL NOTES - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS).

STEP 9: MILL ROADWAY AND BRIDGE PAVEMENT TO SPECIFIED DEPTHS.

STEP 10: PAVE TOP COURSE ON ROADWAY AND BRIDGE.

STEP 11: CUT PAVEMENT FULL DEPTH AT 10" FROM THE CENTER OF THE JOINT (BOTH SIDES OF JOINT) AND REMOVE ALL PAVEMENT MATERIAL BETWEEN THE SAW-CUTS.

STEP 12: INSTALL FINAL ASPHALTIC PLUG JOINT SYSTEM.

SECTION - INSTALLATION OF ASPHALTIC PLUG JOINT WITH BRIDGING PLATE F-

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-9_sb_00630702_Asphaltic_Plug_Joint_Details_4.dgn

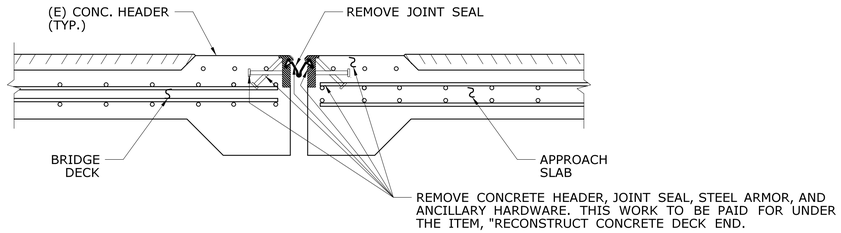
SIGNATURE/BLOCK: OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION
MILLING AND RESURFACING
OF INTERSTATE 91

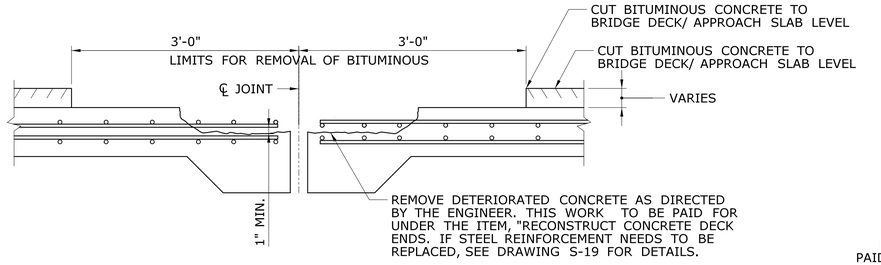
TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

DRAWING TITLE:
ASPHALTIC PLUG EXP. JOINT SYSTEM DETAILS 4

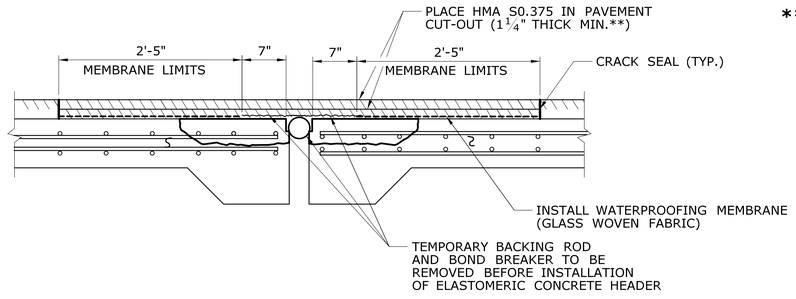
PROJECT NO. 063-702
DRAWING NO. S-9
SHEET NO.



EXISTING CONDITION

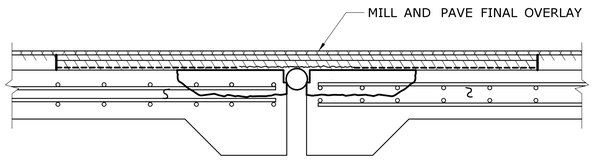


HEADER AND PAVEMENT REMOVAL (STEPS 1-4)

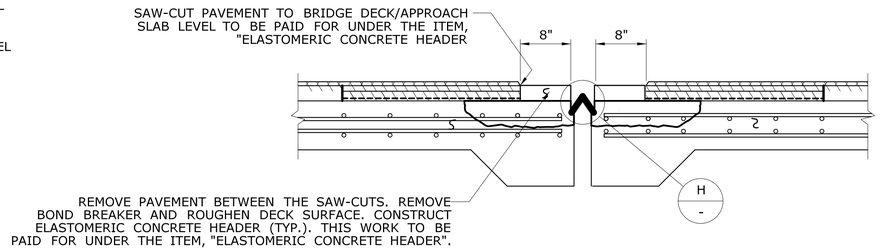


** LIFT THICKNESS TO BE ADJUSTED TO MATCH ADJACENT PAVEMENT THICKNESS (SEE STEP 9 OF SEQUENCE OF WORK)

PLACEMENT OF PAVEMENT IN CUTOUT (STEPS 5 TO 9)



PLACEMENT OF PAVEMENT IN CUTOUT (STEP 10)



PLACEMENT OF PAVEMENT IN CUTOUT (STEPS 11 TO 13)

SUGGESTED SEQUENCE OF WORK

STEP 1: CONTRACTOR SHALL PERFORM AN EXPLORATION AT THE GUTTERLINE TO DETERMINE THE DEPTH OF PAVEMENT AND THE LOCATION OF THE DECK END (CENTERLINE OF THE PROPOSED JOINT) BEFORE PROCEEDING TO STEP 2.

STEP 2: SAW-CUT BITUMINOUS PAVEMENT ON BOTH SIDES OF THE EXISTING JOINT FOR PAVEMENT CUT-OUT. EACH SAW CUT LINE SHALL BE 3' FROM THE END OF THE DECKS. SAW-CUT SHALL NOT DAMAGE EXISTING DECK.

STEP 3: REMOVE EXISTING PAVEMENT MATERIAL WITHIN THE LIMITS SHOWN ON THE CONTRACT SHEET.

STEP 4: REMOVE THE CONCRETE HEADER, JOINT SEAL, STEEL ARMOR AND ANY ANCILLARY HARDWARE. REMOVE SUFFICIENT CONCRETE TO CREATE A 1" CLEARANCE BELOW THE TOP MAT OF THE STEEL REINFORCEMENT. REMOVE ANY ADDITIONAL DETERIORATED CONCRETE AS DIRECTED BY THE ENGINEER OR AS NECESSARY TO CREATE A "STEP" AS SHOWN IN DETAIL'H' ON SHEET S-13. IF THE STEEL REINFORCEMENT HAS SECTION LOSS IN EXCESS OF 20% OF THE CROSS-SECTIONALAREA, REPLACE THE STEEL REINFORCEMENT IN ACCORDANCE WITH THE DETAILS AS SHOWN ON SHEET S-11 AND S-12 .

STEP 5: PLACE CONCRETE IN THE HEADER BLOCKOUT TO THE BRIDGE DECK ELEVATION. CREATE A ½" STEP AS SHOWN IN DETAIL H-13.

STEP 6: INSTALL WATERPROOFING MEMBRANE (GLASS WOVEN FABRIC) ON THE TOP OF DECK WITHIN THE LIMITS SHOWN. SEE DETAIL 'J' ON SHEET S-13.

STEP 7: INSTALL THE TEMPORARY BACKER ROD AND THE BOND BREAKER AT THE LOCATION OF THE PROPOSED ELASTOMERIC CONCRETE HEADER. THIS WORK TO BE PAID FOR UNDER ITEM "ELASTOMERIC CONCRETE HEADER"

STEP 8: PLACE CRACK SEALANT ON VERTICAL EDGE OF PAVEMENT ALONG SAW-CUT LINES. FOR STAGED CONSTRUCTION SEE DETAIL 'K' ON SHEET S-13.

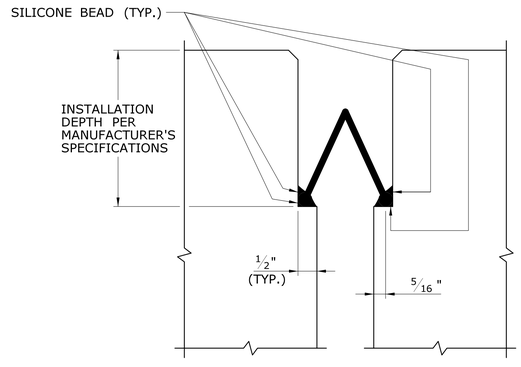
STEP 9: PLACE HMA S0.375 IN THE PAVEMENT CUTOUT. THE FIRST PAVEMENT LIFT SHALL BE 1¼" THICK. ADDITIIONAL LIFTS SHALL BE 1¼" TO 2½" THICK. MATCH THE ELEVATION OF THE EXISTING PAVEMENT. (REFER TO GENERAL NOTES - APJ BITUMINOUS CONCRETE PLACEMENT REQUIREMENTS).

STEP 10: MILL AND PAVE THE FINAL OVERLAY.

STEP 11: CUT PAVEMENT FULL DEPTH, 8" FROM BOTH DECK ENDS. REMOVE ALL PAVEMENT MATERIAL BETWEEN SAW-CUTS, THEN REMOVE THE TEMPORARY BACKER ROD AND THE BOND BREAKER. THIS WORK TO BE PAID FOR UNDER ITEM, "ELASTOMERIC CONCRETE HEADER".

STEP 12: INSTALL THE PROPOSED ELASTOMERIC CONCRETE HEADER.

STEP 13: INSTALL THE PREFORMED SILICONE JOINT SEAL. THIS WORK TO BE PAID FOR UNDER ITEM "PREFORMED SILICONE JOINT SEAL".



G
-
BRIDGE NO. 05882

H
-
PREFORMED SILICONE JOINT SEAL

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-10..sb..0063-0702..Bridge..No..05882..Expansion..Joint..Details.dgn

SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION
MILLING AND RESURFACING
OF INTERSTATE 91

TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

DRAWING TITLE:
BRIDGE NO. 05882
EXP. JOINT DETAILS

PROJECT NO. 63-702
DRAWING NO. S-10
SHEET NO.

NOTES ON STEEL REINFORCEMENT

NEW STEEL REINFORCEMENT WILL MATCH THE EXISTING REBAR FOR SIZE AND SPACING. THE CONTRACTOR MAY USE DRILLED AND GROUTED HOOKED DOWELS; REINFORCEMENT WITH COUPLERS; OR LAP SPLICES OF SUFFICIENT LENGTH.

DRILLED AND GROUTED DOWELS: DRILLED HOLES DIA. AND EMBEDMENT DEPTH PER THE MANUFACTURER'S SPECIFICATIONS TO OBTAIN 100% DEVELOPMENT. MINIMUM CONCRETE COVERAGE IS 2 INCHES. THE MINIMUM HORIZONTAL LENGTH OF THE HOOK IS AS FOLLOWS:

#4 - 6.0" #5 - 7.5" #6 - 9.0" #7 - 10.5" #8 - 12.0"

COUPLERS: MINIMUM CONCRETE COVERAGE IS 2 INCHES. THE RESISTANCE OF A FULL-MECHANICAL CONNECTION SHALL NOT BE LESS THAN 125 PERCENT OF THE SPECIFIED YIELD STRENGTH OF THE BAR IN TENSION OR COMPRESSION, AS REQUIRED

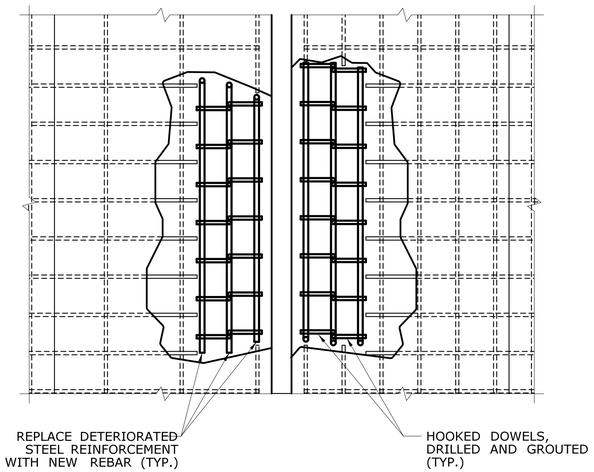
LAP SPLICES: MINIMUM LAP SPLICES ARE AS FOLLOWS: (CLASS C TENSION SPLICE)
FOR REBAR SPACED LATERALLY 6 INCHES OR MORE:

#4: 1'-5"" #5: 1'-9" #6: 2-1" #7: 2-5" #8: 2'-9"

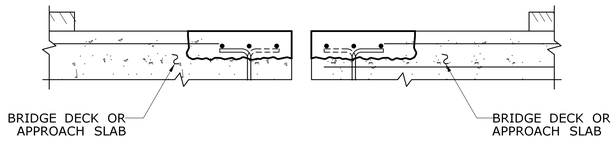
FOR REBAR SPACED LATERALLY LESS THAN 6 INCHES:

#4: 1'-9" #5: 2'-2" #6: 2-7" #7: 3'-3" #8: 4'-3"

STEEL REINFORCEMENT, REBAR COUPLERS, DRILLING AND GROUTING OF DOWELS TO BE PAID UNDER ITEM "RECONSTRUCT CONCRETE DECK ENDS".

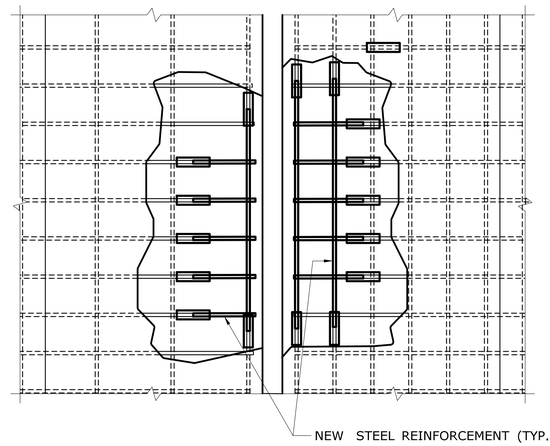


PLAN

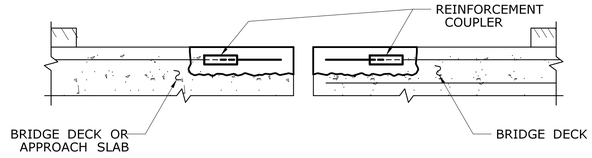


ELEVATION

DRILLED AND GROUTED DOWELS



PLAN



ELEVATION

REINFORCEMENT W/COUPLERS

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-11_sb_00630702_Deteriorated_Steel_Reinforcement_Details.dgn

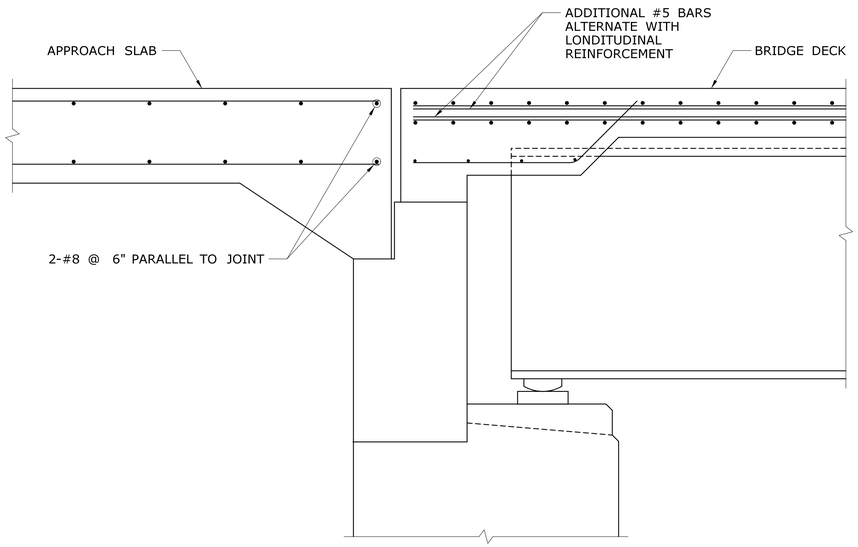
SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION
MILLING AND RESURFACING
Of INTERSTATE 91

TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

DRAWING TITLE:
DETERIORATED STEEL REINFORCEMENT DETAILS

PROJECT NO. 63-702
DRAWING NO. S-11
SHEET NO.



EXISTING STEEL REINFORCEMENT IN ABUTMENT/APPROACH SLABS

EXISTING STEEL REINFORCEMENT IN ABUTMENTS/APPROACH SLABS

BRIDGE NO.	APPROACH SLAB				DECK SLAB			
	LONGITUDINAL		TRANSVERSE		LONGITUDINAL		TRANSVERSE	
	TOP	BOTTOM	TOP	BOTTOM	TOP	BOTTOM	TOP	BOTTOM
05862	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 7"	#5 AT 6"	#5 AT 6'
					#5 X 7'-6"	#5 X 7'-6"		
05863	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 7"	#5 AT 6"	#5 AT 6'
					#5 X 7'-6"	#5 X 7'-6"		
05864	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 9.5"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 x 10"-0"		
05866	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 10.75"	#5 AT 6.5"	#5 AT 6.5"
					#5 X 10'-0"	#5 x 10"-0"		
05881A	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 6"	#5 AT 6.5"	#5 AT 6.5"
					#5 X 10'-0"	#5 x 10"-0"		
05881B	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 6"	#5 AT 6.5"	#5 AT 6.5"
					#5 X 10'-0"	#5 x 10"-0"		
05882	#5 U-SHAPE BAR @ 12"		2 - #6	2 - #6	#4 AT 12"	#5 AT 9"	#5 AT 7"	#5 AT 7"
	BACKWALL W/O APPROACH SLAB				#5 X 13'-0"	#5 X 13'-0"		
05924	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 7"	#5 AT 7"	#5 AT 7"
					#5 X 7'-6"	#5 X 7'-6"		
05976A	NO APPROACH SLAB		NO APPROACH SLAB		#4 AT 12"	#5 AT 9"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 X 10'-0"		
05976B	NO APPROACH SLAB		NO APPROACH SLAB		#4 AT 12"	#5 AT 9"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 X 10'-0"		
05977A	NO APPROACH SLAB		NO APPROACH SLAB		#4 AT 12"	#6 AT 9"	#6 AT 6"	#6 AT 6"
					#5 X 10'-0"	#5 X 10'-0"		
05977B	NO APPROACH SLAB		NO APPROACH SLAB		#4 AT 12"	#6 AT 9"	#6 AT 6"	#6 AT 6"
					#5 X 10'-0"	#5 X 10'-0"		
05979A	NO APPROACH SLAB		NO APPROACH SLAB		#4 AT 12"	#5 AT 9"	#6 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 X 10'-0"		
05979B	NO APPROACH SLAB		NO APPROACH SLAB		#4 AT 12"	#5 AT 9"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 X 10'-0"		
05994	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 9.5"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 x 10"-0"		
05995	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 10.75"	#5 AT 6.5"	#5 AT 6.5"
					#5 X 10'-0"	#5 x 10"-0"		
06008	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 7"	#5 AT 7"	#5 AT 7"
					#5 X 7'-6"	#5 X 7'-6"		
06040A	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 7"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 x 10"-0"		
06040B	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 10"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 x 10"-0"		
06151	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 7"	#5 AT 7"	#5 AT 7"
					#5 X 7'-6"	#5 X 7'-6"		
06218	#9 AT 6"	#9 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 10"	#5 AT 7"	#5 AT 7"
					#5 X 10'-0"	#5 X 10'-0"		
06219	#8 AT 6"	#8 AT 6"	#5 AT 12"	#5 AT 12"	#4 AT 12"	#5 AT 9"	#5 AT 6"	#5 AT 6"
					#5 X 10'-0"	#5 X 10'-0"		

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-12_sb_00630702_Existing_Deck_End_Steel_Reinforcement.dgn

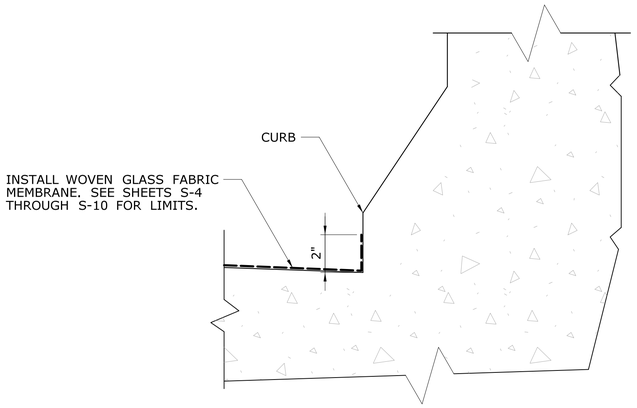
SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION
MILLING AND RESURFACING
OF INTERSTATE 91

TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

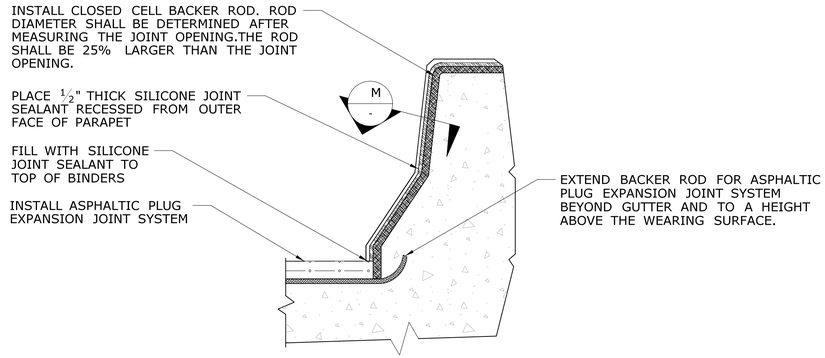
DRAWING TITLE:
EXISTING DECK END
STEEL REINFORCEMENT

PROJECT NO. 63-702
DRAWING NO. S-12
SHEET NO.



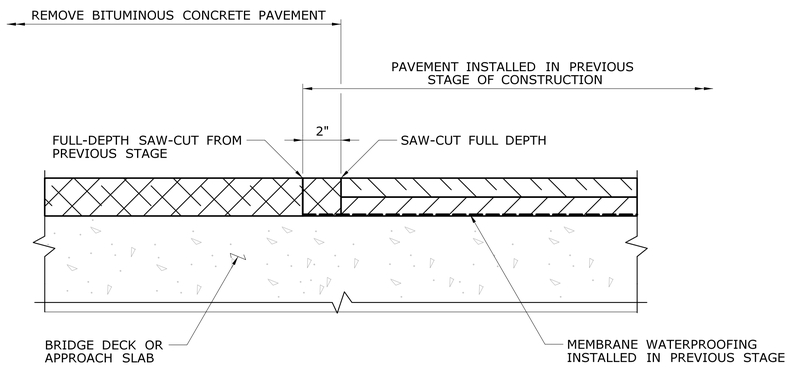
SECTION - GUTTERLINE AT PAVEMENT "CUTOUT" — **J**

N.T.S.



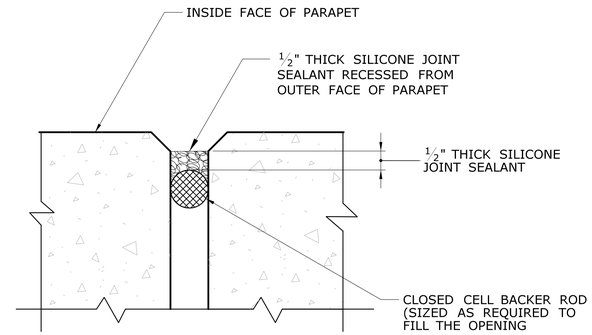
SECTION - PARAPET JOINT TREATMENT WITH ASPHALTIC PLUG JOINT SYSTEM — **L**

N.T.S.



INITIAL LONGITUDINAL STAGE CONSTRUCTION JOINT IN PAVEMENT "CUTOUT" — **K** / **-**

N.T.S.



SECTION - JOINT TREATMENT AT PARAPET — **M**

N.T.S.

NOTES FOR SEALING JOINTS

1. ANY EXISTING BACKER ROD AND JOINT SEALANT SHALL BE COMPLETELY REMOVED.
2. SURFACES OF CONCRETE ALONG JOINT SHALL BE CLEANED BY ABRASIVE BLAST CLEANING. SURFACES TO WHICH SILICONE SEALANT WILL ADHERE SHALL BE FREE OF DUST AND LOOSE OR DETERIORATED CONCRETE BEFORE INSTALLING BACKER ROD AND SILICONE JOINT SEAL.
3. COST FOR SEALING PARAPET JOINTS TO BE INCLUDED FOR PAYMENT WITH "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: JPC
CHECKED BY: JRH
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-13_sb_00630702_Crack_Sealant_and_Parapet_Joint_Details.dgn

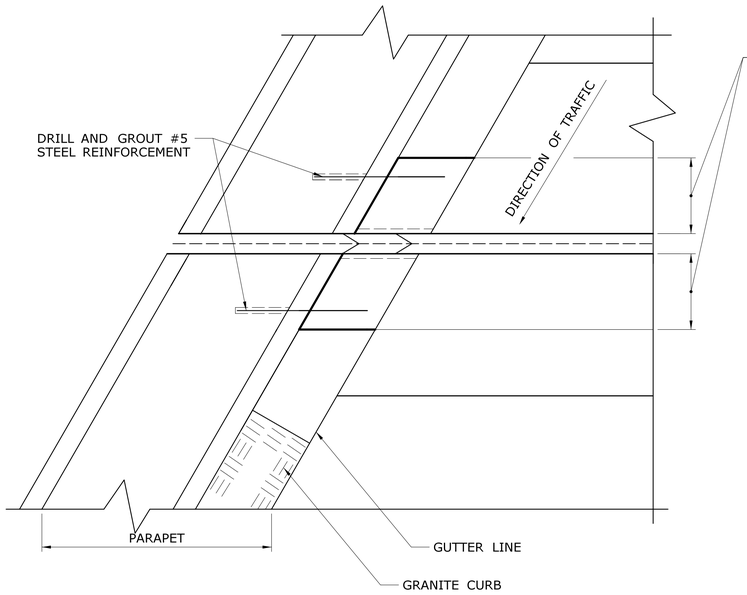
SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
PAVEMENT PRESERVATION MILLING AND RESURFACING OF INTERSTATE 91

TOWN:
CITY OF HARTFORD
TOWN OF WINDSOR

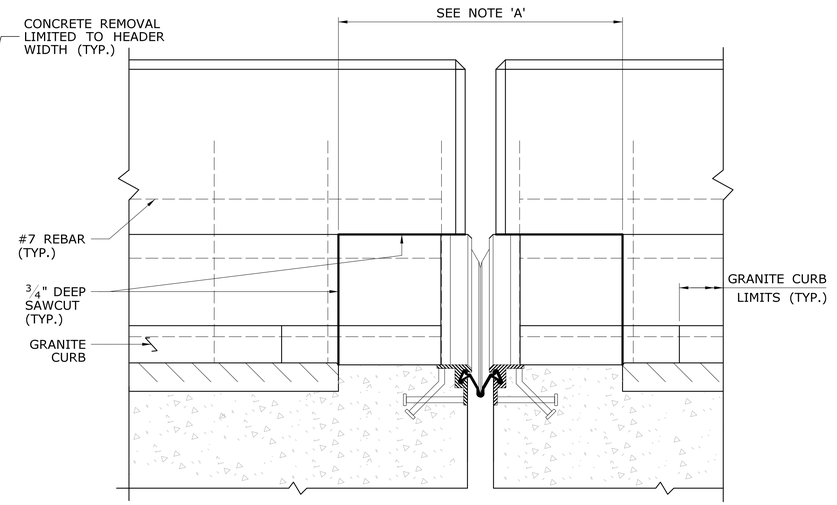
DRAWING TITLE:
CRACK SEAL DETAILS & PARAPET JOINT DETAILS

PROJECT NO. **63-702**
DRAWING NO. **S-13**
SHEET NO.



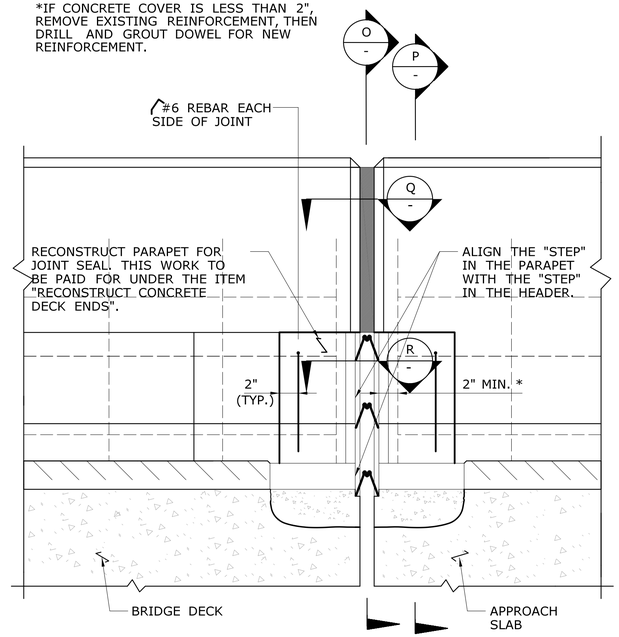
PARTIAL PLAN - JOINT TREATMENT IN PARAPET

NOTE 'A':
REMOVE CONCRETE WITHIN THE LIMITS OF THE EXISTING HEADER. REMOVE THE JOINT SEAL AND STEEL EXTRUSIONS. TO BE PAID FOR UNDER PAY ITEM "RECONSTRUCT CONCRETE DECK ENDS".



EXISTING

*IF CONCRETE COVER IS LESS THAN 2", REMOVE EXISTING REINFORCEMENT, THEN DRILL AND GROUT DOWEL FOR NEW REINFORCEMENT.

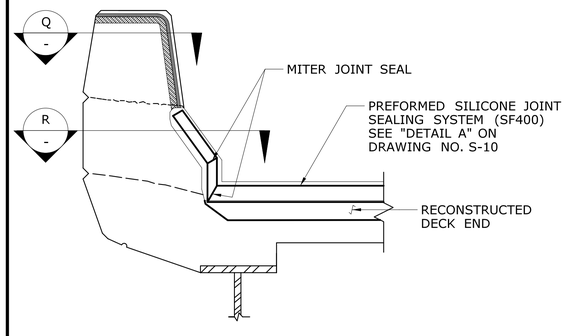


PROPOSED

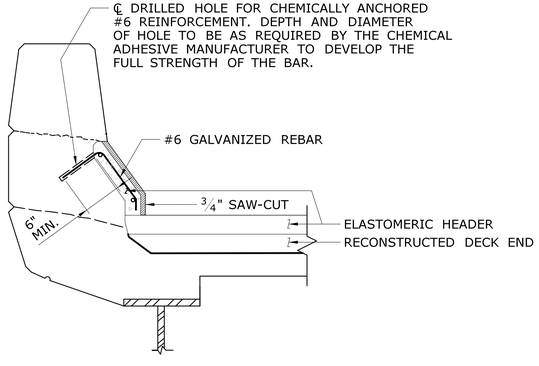
N - JOINT TREATMENT IN PARAPET

NOTE:

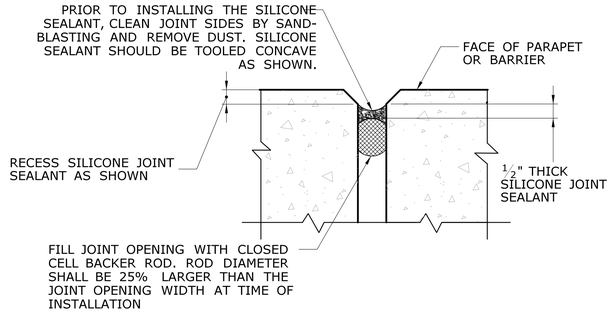
PRIOR TO INSTALLING THE NEW BACKER ROD AND SILICONE JOINT SEALANT, REMOVE EXISTING JOINT MATERIAL. CLEAN JOINT SIDES BY SANDBLASTING. DUST SHALL BE REMOVED BY THE METHOD APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR UNDER THE ITEM "REPLACE JOINT SEAL".



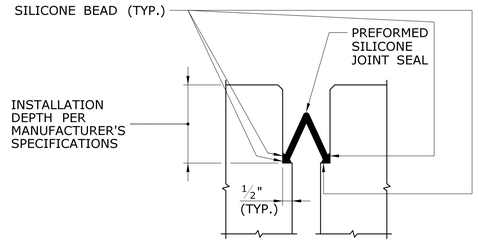
O - SECTION - PARAPET JOINT SEAL



P - SECTION - PARAPET RECONSTRUCTION



Q - SECTION THROUGH PARAPET OR MEDIAN BARRIER JOINT



R - SECTION THROUGH PARAPET OR SIDEWALK

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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Plotted Date: 12/12/2014

DESIGNER/DRAFTER: MJPL
CHECKED BY: RPL
NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\S-14..sb..0063-0702..Bridge No..05882..Parapet..Joint..Seal..Details.dgn

SIGNATURE/ BLOCK: OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE: PAVEMENT PRESERVATION MILLING AND RESURFACING OF INTERSTATE 91

TOWN: CITY OF HARTFORD TOWN OF WINDSOR

DRAWING TITLE: BR. NO. 05882 PARAPET JOINT SEAL DETAILS

PROJECT NO. 63-702
DRAWING NO. S-14
SHEET NO.